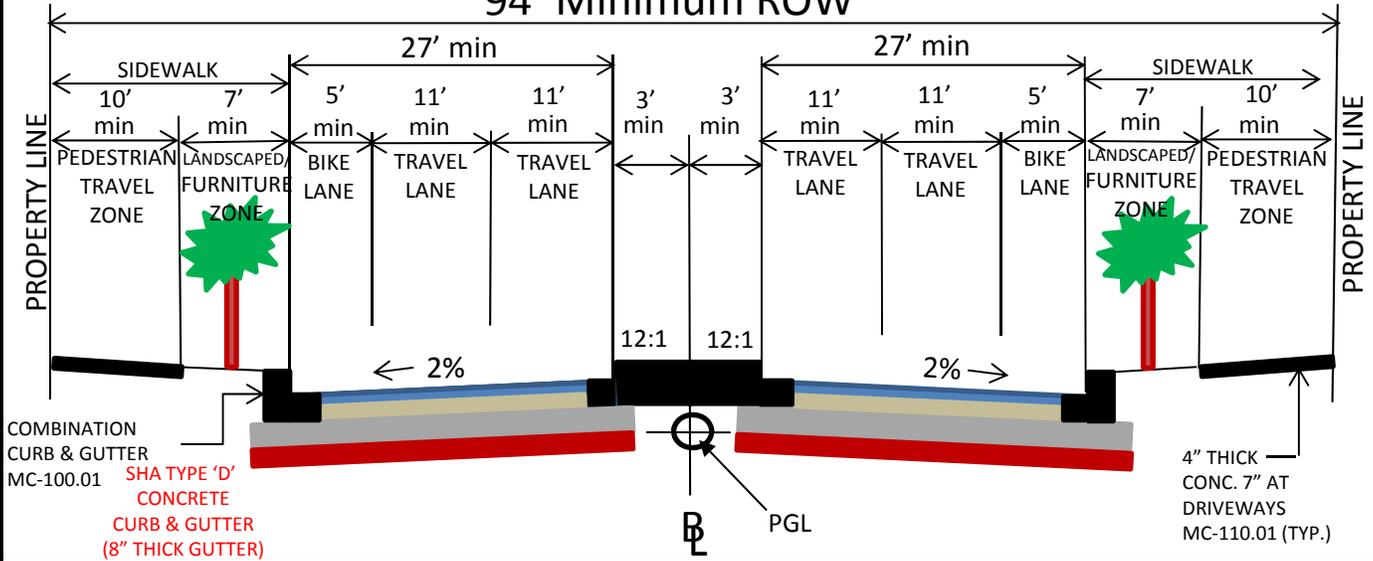


**Design Data**

NOT TO SCALE

**94' Minimum ROW**



Paving Detail		Design Data		
3" BITUMINOUS CONCRETE SURFACE COURSE IN TWO 1 1/2" LAYERS	TARGET SPEED (MPH)	SUPERELEVATION DISTRIBUTION		MAX GRADE
	25	NONE (-2% MAX)		8%
5" BITUMINOUS CONCRETE BASE COURSE	30	AASHTO METHOD 2 (4% MAX)		8%
	MIN ALLOWABLE $\text{R}$ RADIUS = 300'			
8" GRADED AGGREGATE BASE COURSE IN TWO 4" LAYERS				
APPROVED SUBGRADE				

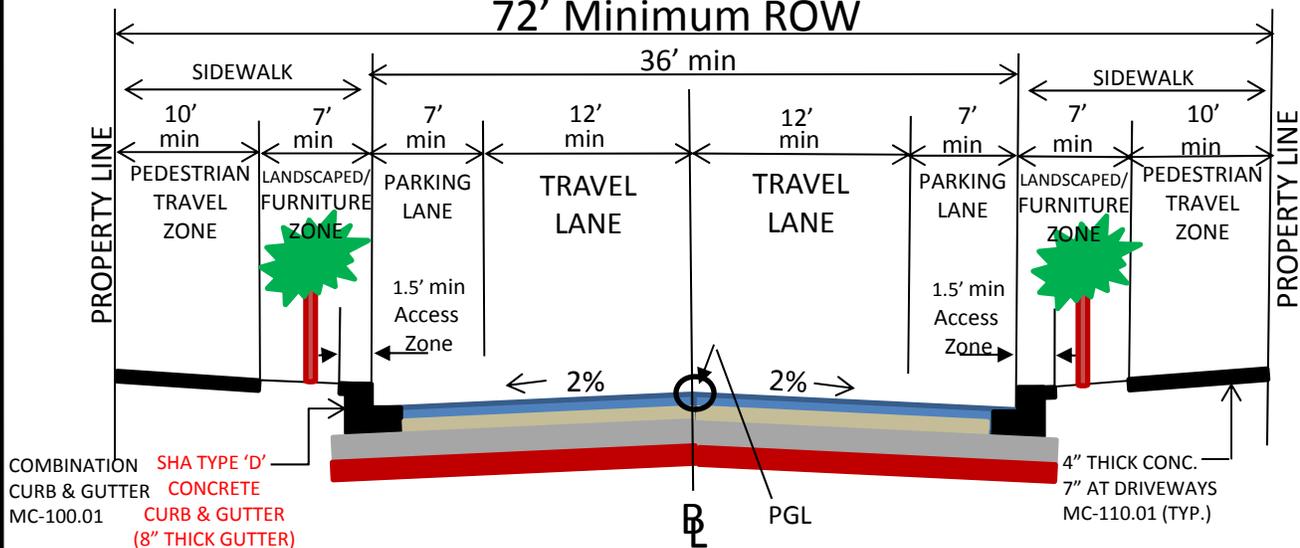
- General Notes**
1. LATEST EDITION OF THE MARYLAND STATE HIGHWAY ADMINISTRATION SPECIFICATIONS WILL APPLY FOR MATERIALS AND METHODS OF CONSTRUCTION.
  2. RIGHT-OF-WAY (R-O-W) AND PUBLIC UTILITY EASEMENTS (PUEs) ARE SUBJECT TO "DECLARATIONS OF TERMS AND PROVISIONS OF PUBLIC UTILITY EASEMENTS" RECORDED AS LIBER 3834, FOLIO 457 IN THE LAND RECORDS OF MONTGOMERY COUNTY.
  3. STANDARD ELEVATION AT PROPERTY LINE, RELATIVE TO PGL SHALL NOT VARY AT DRIVEWAYS.
  4. PAVING DETAIL DEPICTS THE MINIMUM REQUIRED SECTION WHICH MUST BE CERTIFIED BY A QUALIFIED PROFESSIONAL TO THE SATISFACTION OF THE DIRECTOR. AN ENGINEERED PAVING DESIGN USING SHA METHODOLOGY WILL BE REQUIRED WHEN THE MINIMUM PAVING DETAIL CANNOT BE SUPPORTED.
  5. ADDITIONAL R-O-W MAY BE NEEDED AT INTERSECTIONS WHERE ONE OR MULTIPLE TURN LANES ARE NECESSARY TO MAINTAIN ACCEPTABLE LEVELS-OF-SERVICE, ADDITIONAL BICYCLE FACILITIES RECOMMENDED UNDER THE BIKEWAY MASTER PLAN, PARKING IF THE GEOMETRY DOES NOT CREATE A SAFETY PROBLEM OR REDUCE OVERALL TRAFFIC OPERATIONS, UTILITIES, OR STORMWATER MANAGEMENT FACILITIES WHICH MAY INCLUDE BIORETENTION AREAS AS APPROVED BY THE DIRECTOR.
  6. SELECTION OF APPROPRIATE TARGET SPEED (EQUAL OR SLIGHTLY LESS THAN DESIGN SPEED) IS SUBJECT TO DEPARTMENT REVIEW AND APPROVAL. APPROVED TARGET AND DESIGN SPEED MUST APPEAR ON CONSTRUCTION DRAWINGS.
  7. ALL UNPAVED AREAS WITHIN THE LIMITS OF DISTURBANCE SHALL BE STABILIZED WITH ESTABLISH GRASS TURF (SOD) OR APPROVED LANDSCAPING.
  8. STREET TREES OF APPROVED TYPE, SIZE AND SPACING SHALL BE PLANTED AT LOCATIONS SHOWN.

STANDARD DEVELOPED AND IMPLEMENTED IN CONFORMANCE WITH ROAD CODE REVISION ADOPTED (DATE)		CITY OF ROCKVILLE DEPARTMENT OF PUBLIC WORKS
_____ DIRECTOR, DEPARTMENT OF PUBLIC WORKS	_____ DATE	
_____ CHIEF, ENGINEERING DIVISION	_____ DATE	
_____ CHIEF, TRAFFIC AND TRANSPORTATION DIVISION	_____ DATE	BUSINESS DISTRICT ROAD CLASS I
		STANDARD DETAIL

**Design Data**

NOT TO SCALE

**72' Minimum ROW**



Paving Detail		Design Data		
3" BITUMINOUS CONCRETE SURFACE COURSE IN TWO 1 1/2" LAYERS	TARGET SPEED (MPH)	SUPERELEVATION DISTRIBUTION		MAX GRADE
	25	NONE (-2% MAX)		8%
5" BITUMINOUS CONCRETE BASE COURSE	30	AASHTO METHOD 2 (4% MAX)		8%
8" GRADED AGGREGATE BASE COURSE IN TWO 4" LAYERS	MIN ALLOWABLE $R_c$ RADIUS = 300'			
APPROVED SUBGRADE				

GEOMETRIC DESIGN SHALL CONFORM TO THE LATEST EDITION OF THE AASHTO "GREEN BOOK" GUIDELINES.

**General Notes**

- LATEST EDITION OF THE MARYLAND STATE HIGHWAY ADMINISTRATION SPECIFICATIONS WILL APPLY FOR MATERIALS AND METHODS OF CONSTRUCTION.
- RIGHT-OF-WAY (R-O-W) AND PUBLIC UTILITY EASEMENTS (PUEs) ARE SUBJECT TO "DECLARATIONS OF TERMS AND PROVISIONS OF PUBLIC UTILITY EASEMENTS" RECORDED AS LIBER 3834, FOLIO 457 IN THE LAND RECORDS OF MONTGOMERY COUNTY.
- STANDARD ELEVATION AT PROPERTY LINE, RELATIVE TO PGL SHALL NOT VARY AT DRIVEWAYS.
- PAVING DETAIL DEPICTS THE MINIMUM REQUIRED SECTION WHICH MUST BE CERTIFIED BY A QUALIFIED PROFESSIONAL TO THE SATISFACTION OF THE DIRECTOR. AN ENGINEERED PAVING DESIGN USING SHA METHODOLOGY WILL BE REQUIRED WHEN THE MINIMUM PAVING DETAIL CANNOT BE SUPPORTED.
- ADDITIONAL R-O-W MAY BE NEEDED AT INTERSECTIONS WHERE ONE OR MULTIPLE TURN LANES ARE NECESSARY TO MAINTAIN ACCEPTABLE LEVELS-OF-SERVICE, WIDER LANE WIDTHS FOR GEOMETRIC PURPOSES AND/OR HIGH PARKING TURNOVER, BICYCLE FACILITIES RECOMMENDED UNDER THE BIKEWAY MASTER PLAN, UTILITIES, OR STORMWATER MANAGEMENT FACILITIES WHICH MAY INCLUDE BIORETENTION AREAS AND ARE SUBJECT TO THE APPROVAL OF THE DIRECTOR. PARKING CAN BE REMOVED IF THE GEOMETRY AND SPACING OF ACCESS POINTS ALONG THE ROAD CREATES A SAFETY HAZARD OR A MAINTENANCE ISSUE. THEREFORE THE R-O-W AND THE PAVING WIDTH MAY BE REDUCED BY UP TO 14 FEET UPON APPROVAL OF THE DIRECTOR.
- SELECTION OF APPROPRIATE TARGET SPEED (EQUAL OR SLIGHTLY LESS THAN DESIGN SPEED) IS SUBJECT TO DEPARTMENT REVIEW AND APPROVAL. APPROVED TARGET AND DESIGN SPEED MUST APPEAR ON CONSTRUCTION DRAWINGS.
- ALL UNPAVED AREAS WITHIN THE LIMITS OF DISTURBANCE SHALL BE STABILIZED WITH ESTABLISH GRASS TURF (SOD) OR APPROVED LANDSCAPING.
- STREET TREES OF APPROVED TYPE, SIZE AND SPACING SHALL BE PLANTED AT LOCATIONS SHOWN.

STANDARD DEVELOPED AND IMPLEMENTED IN CONFORMANCE WITH ROAD CODE REVISION ADOPTED (DATE)

\_\_\_\_\_  
 DIRECTOR, DEPARTMENT OF PUBLIC WORKS      DATE

\_\_\_\_\_  
 CHIEF, ENGINEERING DIVISION      DATE

\_\_\_\_\_  
 CHIEF, TRAFFIC AND TRANSPORTATION DIVISION      DATE

CITY OF ROCKVILLE  
 DEPARTMENT OF PUBLIC WORKS

BUSINESS DISTRICT ROAD  
 CLASS II

STANDARD DETAIL